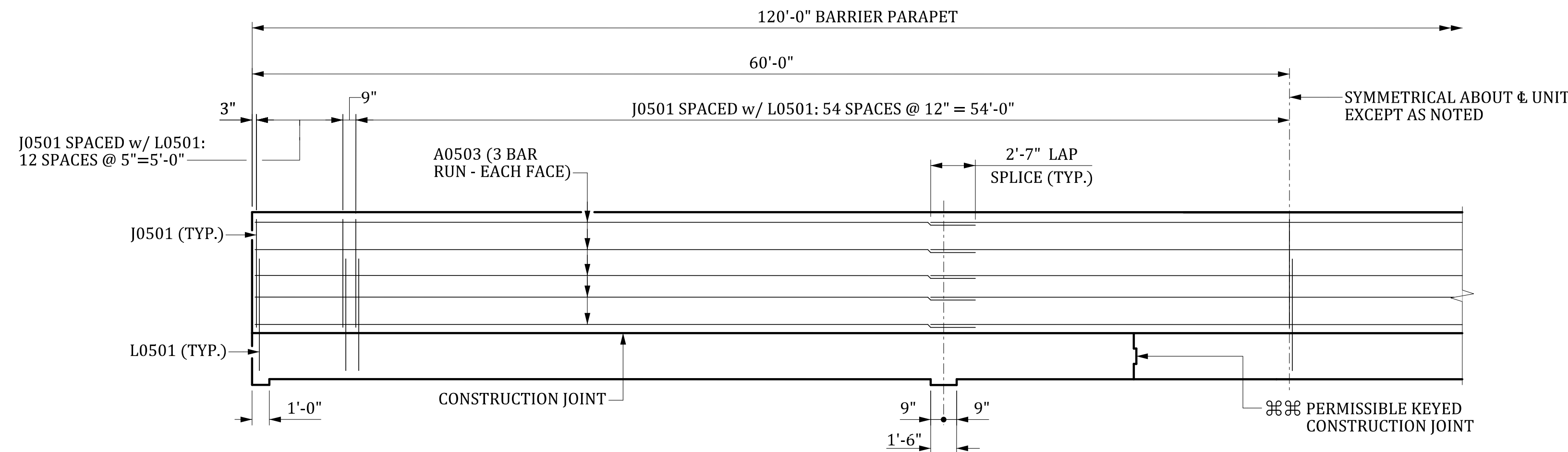


PARTIAL PLAN



PARTIAL ELEVATION

REINFORCING STEEL SCHEDULE							
MARK	NO.	REQ'D	DIMENSION				LENGTH
			"a"	"b"	"c"	"d"	
A0401	8	19'-2"					19'-2"
A0501	244	35'-11"					35'-11"
A0502	78	25'-1"					25'-1"
A0503	60	41'-8"					41'-8"
A0701	36	27'-0"					27'-0"
A0801	39	42'-0"					42'-0"
A0901	78	42'-1"					42'-1"
A1001	78	40'-3"					40'-3"
A1002	72	22'-0"					22'-0"
A1003	72	33'-4"					33'-4"
B0501	134	5'-0"	7"				5'-7"
J0501	270	5 1/2"	3'-3"				7'-0"
K0401	40	4 1/2"	1'-6"	1'-2"			4'-11"
KB0401	40	4 1/2"	1'-6"	8"			4'-5"
L0501	270	10"	3'-7"	6 1/4"	3'-7"		8'-7"
SB		2" HT.	AS NECESSARY				
CHCU		1 1/4" HT.	AS NECESSARY				
ESTIMATED QUANTITIES							
ITEM	UNIT	120' SPAN					
2.0" SCH. 80 PVC CONDUIT	LF	480.0					
CONCRETE, CLASS 4000	CY	270.6					
REINFORCING STEEL	LB	67,480					
MASH CONCRETE BARRIER PARAPET	LF	240.0					
ELASTOMERIC BEARING ASSEMBLY (FLAT SLAB)	EA	4					

- NOTES:
- CAST THE 120' CONTINUOUS UNIT IN ONE POUR FROM ONE END OF SPAN TO THE OTHER USING A SUITABLE SCREED. USE AN APPROVED RETARDING AGENT AND ESTABLISH AND MAINTAIN A MINIMUM POURING RATE OF 55 CY PER HOUR UNLESS APPROVED OTHERWISE BY THE RCE. IF A POURING RATE OF 55 CY PER HOUR IS NOT POSSIBLE, CAST THE 120' CONTINUOUS UNIT USING THE PERMISSIBLE KEYED CONSTRUCTION JOINT SHOWN.
 - DO NOT REMOVE SLAB FALSEWORK UNTIL THE ENTIRE 120' SPAN UNIT HAS BEEN POURED AND CURED ACCORDING TO SECTION 702 OF THE STANDARD SPECIFICATIONS.
 - CAST BARRIER PARAPET CONCRETE AFTER SLAB FALSEWORK HAS BEEN STRUCK.
 - FOR ARMOR PLATE DETAIL, SEE "GENERAL DETAILS" SHEET.
 - FOR TYPICAL SECTION, SECTION THRU BARRIER PARAPET, SLAB BUILD-DOWN DETAILS AND DRAIN DETAILS, SEE "120' FLAT SLAB UNIT (3 SPAN) SUPERSTRUCTURE DETAILS" SHEET.
 - FOR SLAB DRAIN LOCATIONS, SEE "BRIDGE PLAN AND PROFILE" SHEET.
 - FOR REINFORCING BENDING DETAILS, SEE "REINFORCING BENDING DETAILS" SHEETS.
 - ⊗ MASH BARRIER PARAPET
 - ⊗ ONLY ONE CONSTRUCTION JOINT IS ALLOWED IN THE 120' CONTINUOUS UNIT.

REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION

REVIEWED	CTM	GCM	11-23
QUAN.	CTM	MWB	11-23
DR.	SJA	DPP	10-23
DES.	BY	CHK.	DATE

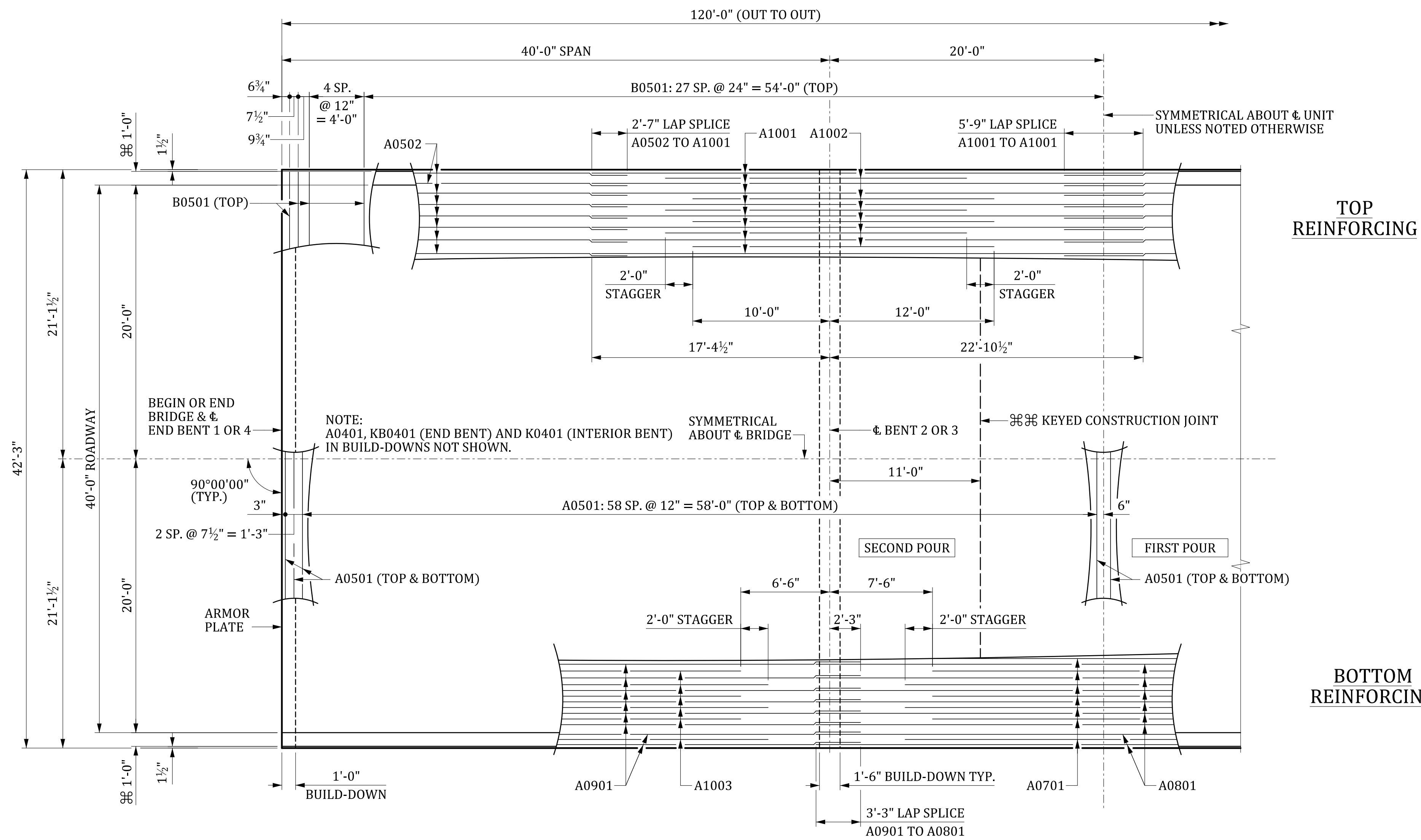
THIS DRAWING IS FURNISHED FOR INFORMATION ONLY. ALL DIMENSIONS SHOWN ARE SHEET SPECIFIC. ANY USE OF THIS DESIGN AND DRAWING, INCLUDING DIMENSIONS, MUST BE CHECKED BY THE USER'S ENGINEER TO ENSURE DESIGN IS ADEQUATE FOR THE INTENDED USE. ALL DRAWINGS MUST BE SIGNED AND SEALED BY A SOUTH CAROLINA REGISTERED PROFESSIONAL ENGINEER WHEN USED.

CONSULTANT NAME/LOGO

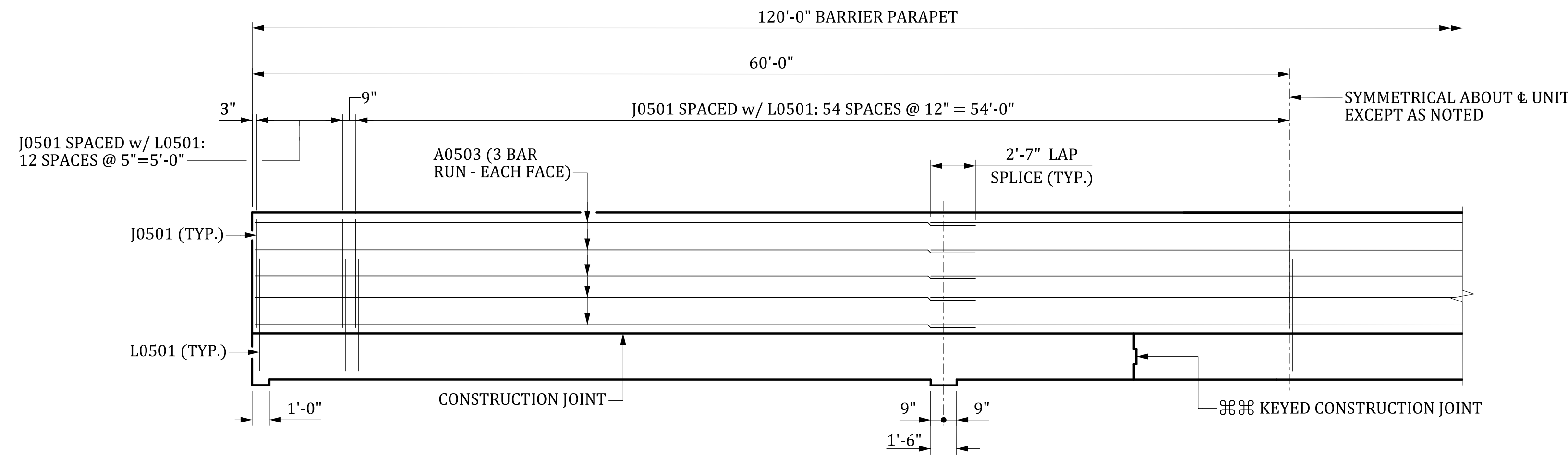
SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

**120' FLAT SLAB UNIT (3 SPAN)
SUPERSTRUCTURE
(34'-0" ROADWAY)**

COUNTY: #### ROUTE: ####



PARTIAL PLAN



PARTIAL ELEVATION

REINFORCING STEEL SCHEDULE							
MARK	NO. REQ'D	DIMENSION					LENGTH
		"a"	"b"	"c"	"d"	"e"	
A0401	8	25'-2"				25'-2"	
A0501	244	41'-11"				41'-11"	
A0502	90	25'-1"				25'-1"	
A0503	60	41'-8"				41'-8"	
A0701	42	27'-0"				27'-0"	
A0801	45	42'-0"				42'-0"	
A0901	90	42'-1"				42'-1"	
A1001	90	40'-3"				40'-3"	
A1002	84	22'-0"				22'-0"	
A1003	84	33'-4"				33'-4"	
B0501	134	5'-0"	7"			5'-7"	
J0501	270	5 1/2"	3'-3"			7'-0"	
K0401	52	4 1/2"	1'-6"	1'-2"		4'-11"	
KB0401	52	4 1/2"	1'-6"	8"		4'-5"	
L0501	270	10"	3'-7"	6 1/4"	3'-7"	8'-7"	
SB		2" HT.				AS NECESSARY	
CHCU		1 1/4" HT.				AS NECESSARY	
ESTIMATED QUANTITIES							
ITEM	UNIT	120' SPAN					
2.0" SCH. 80 PVC CONDUIT	LF	480.0					
CONCRETE, CLASS 4000	CY	315.6					
REINFORCING STEEL	LB	77,087					
MASH CONCRETE BARRIER PARAPET	LF	240.0					
ELASTOMERIC BEARING ASSEMBLY (FLAT SLAB)	EA	4					

NOTES:

CAST THE 120' CONTINUOUS UNIT IN TWO POURS USING A SUITABLE SCREED AND THE KEYED CONSTRUCTION JOINT SHOWN. USE AN APPROVED RETARDING AGENT AND ESTABLISH AND MAINTAIN A MINIMUM POURING RATE OF 45 CY PER HOUR UNLESS APPROVED OTHERWISE BY THE RCE.

DO NOT REMOVE SLAB FALSEWORK UNTIL THE ENTIRE 120' SPAN UNIT HAS BEEN POURED AND CURED ACCORDING TO SECTION 702 OF THE STANDARD SPECIFICATIONS.

CAST BARRIER PARAPET CONCRETE AFTER SLAB FALSEWORK HAS BEEN STRUCK.

FOR ARMOR PLATE DETAIL, SEE "GENERAL DETAILS" SHEET.

FOR TYPICAL SECTION, SECTION THRU BARRIER PARAPET, SLAB BUILD-DOWN DETAILS AND DRAIN DETAILS, SEE "120' FLAT SLAB UNIT (3 SPAN) SUPERSTRUCTURE DETAILS" SHEET.

FOR SLAB DRAIN LOCATIONS, SEE "BRIDGE PLAN AND PROFILE" SHEET.

FOR REINFORCING BENDING DETAILS, SEE "REINFORCING BENDING DETAILS" SHEETS.

⌘ MASH BARRIER PARAPET

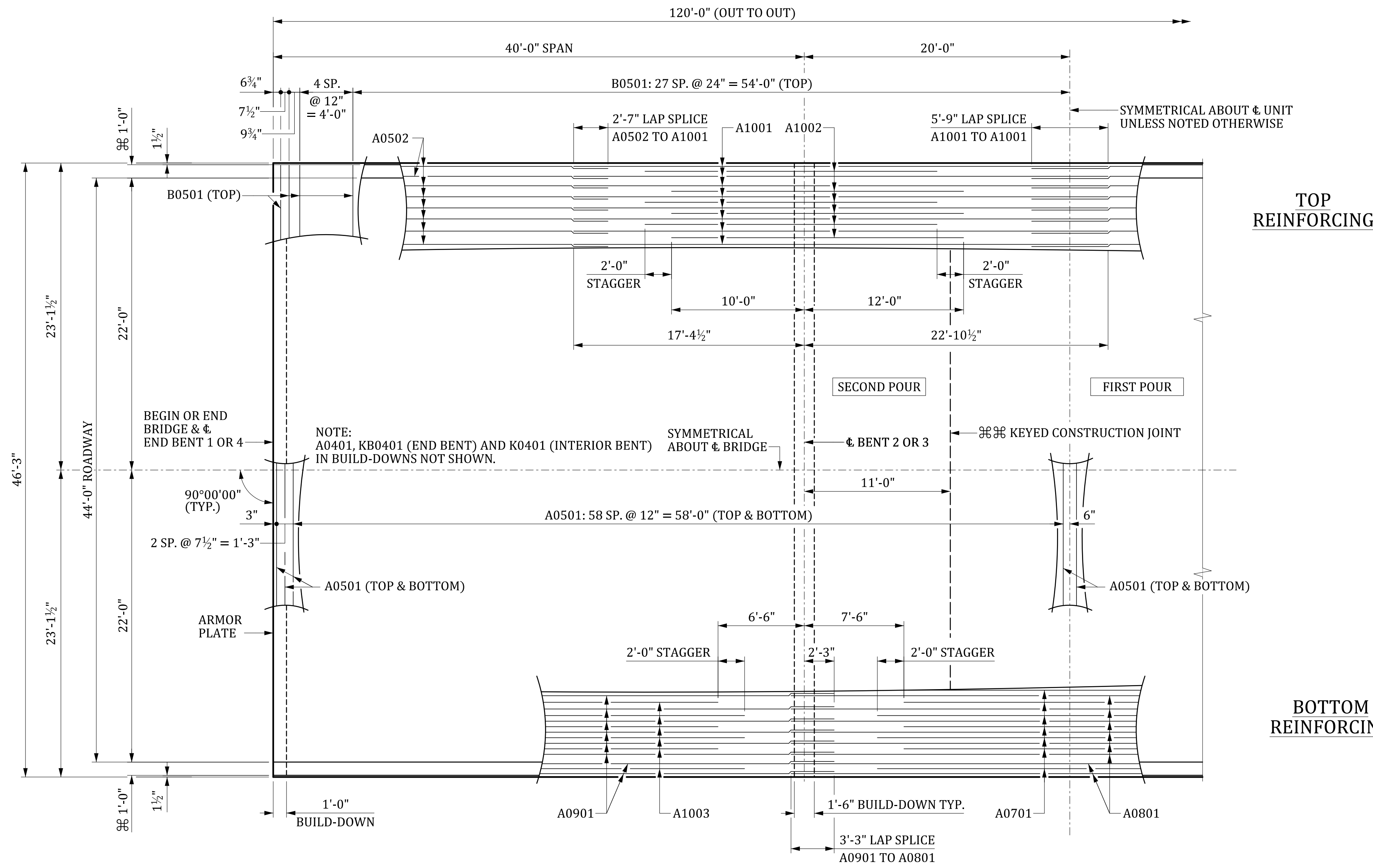
⌘ ONLY ONE CONSTRUCTION JOINT IS ALLOWED IN THE 120' CONTINUOUS UNIT.

THIS DRAWING IS FURNISHED FOR INFORMATION ONLY. ALL DIMENSIONS SHOWN ARE SHEET SPECIFIC. ANY USE OF THIS DESIGN AND DRAWING, INCLUDING DIMENSIONS, MUST BE CHECKED BY THE USER'S ENGINEER TO ENSURE DESIGN IS ADEQUATE FOR THE INTENDED USE. ALL DRAWINGS MUST BE SIGNED AND SEALED BY A SOUTH CAROLINA REGISTERED PROFESSIONAL ENGINEER WHEN USED.

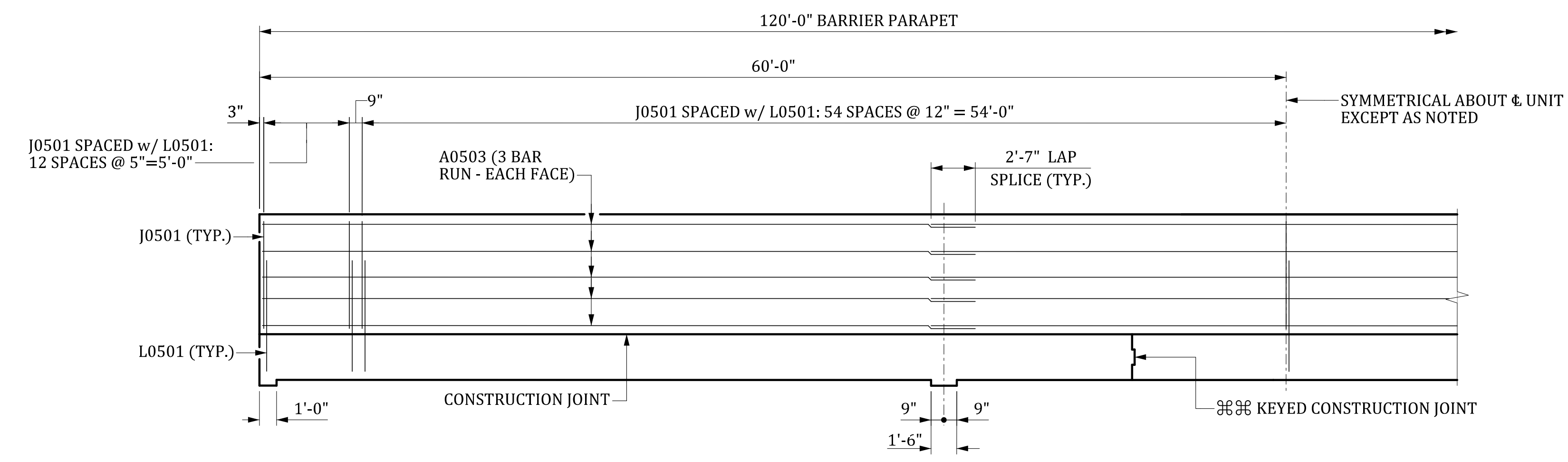
CONSULTANT NAME/LOGO	
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION	
120' FLAT SLAB UNIT (3 SPAN) SUPERSTRUCTURE (40'-0" ROADWAY)	
COUNTY: #####	ROUTE: #####

REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION

REVIEWED	CTM	GCM	11-23
QUAN.	CTM	MWB	11-23
DR.	SJA	DPP	10-23
DES.	BY	CHK.	DATE



PARTIAL PLAN



PARTIAL ELEVATION

REINFORCING STEEL SCHEDULE						
MARK	NO. REQ'D	DIMENSION				LENGTH
		"a"	"b"	"c"	"d"	
A0401	8	29'-2"				29'-2"
A0501	244	45'-11"				45'-11"
A0502	98	25'-1"				25'-1"
A0503	60	41'-8"				41'-8"
A0701	46	27'-0"				27'-0"
A0801	49	42'-0"				42'-0"
A0901	98	42'-1"				42'-1"
A1001	98	40'-3"				40'-3"
A1002	92	22'-0"				22'-0"
A1003	92	33'-4"				33'-4"
B0501	134	5'-0"	7"			5'-7"
J0501	270	5 1/2"	3'-3"			7'-0"
K0401	60	4 1/2"	1'-6"	1'-2"		4'-11"
KB0401	60	4 1/2"	1'-6"	8"		4'-5"
L0501	270	10"	3'-7"	6 1/4"	3'-7"	8'-7"
SB		2" HT.				AS NECESSARY
CHCU		1 1/4" HT.				AS NECESSARY

ESTIMATED QUANTITIES		
ITEM	UNIT	120' SPAN
2.0" SCH. 80 PVC CONDUIT	LF	480.0
CONCRETE, CLASS 4000	CY	345.6
REINFORCING STEEL	LB	83,490
MASH CONCRETE BARRIER PARAPET	LF	240.0
ELASTOMERIC BEARING ASSEMBLY (FLAT SLAB)	EA	4

NOTES:

CAST THE 120' CONTINUOUS UNIT IN TWO POURS USING A SUITABLE SCREED AND THE KEYED CONSTRUCTION JOINT SHOWN. USE AN APPROVED RETARDING AGENT AND ESTABLISH AND MAINTAIN A MINIMUM POURING RATE OF 45 CY PER HOUR UNLESS APPROVED OTHERWISE BY THE RCE.

DO NOT REMOVE SLAB FALSEWORK UNTIL THE ENTIRE 120' SPAN UNIT HAS BEEN POURED AND CURED ACCORDING TO SECTION 702 OF THE STANDARD SPECIFICATIONS.

CAST BARRIER PARAPET CONCRETE AFTER SLAB FALSEWORK HAS BEEN STRUCK.

FOR ARMOR PLATE DETAIL, SEE "GENERAL DETAILS" SHEET.

FOR TYPICAL SECTION, SECTION THRU BARRIER PARAPET, SLAB BUILD-DOWN DETAILS AND DRAIN DETAILS, SEE "120' FLAT SLAB UNIT (3 SPAN) SUPERSTRUCTURE DETAILS" SHEET.

FOR SLAB DRAIN LOCATIONS, SEE "BRIDGE PLAN AND PROFILE" SHEET.

FOR REINFORCING BENDING DETAILS, SEE "REINFORCING BENDING DETAILS" SHEETS.

⊗ MASH BARRIER PARAPET

⊗ ONLY ONE CONSTRUCTION JOINT IS ALLOWED IN THE 120' CONTINUOUS UNIT.

THIS DRAWING IS FURNISHED FOR INFORMATION ONLY. ALL DIMENSIONS SHOWN ARE SHEET SPECIFIC. ANY USE OF THIS DESIGN AND DRAWING, INCLUDING DIMENSIONS, MUST BE CHECKED BY THE USER'S ENGINEER TO ENSURE DESIGN IS ADEQUATE FOR THE INTENDED USE. ALL DRAWINGS MUST BE SIGNED AND SEALED BY A SOUTH CAROLINA REGISTERED PROFESSIONAL ENGINEER WHEN USED.

CONSULTANT NAME/LOGO

SOUTH CAROLINA
 DEPARTMENT OF TRANSPORTATION

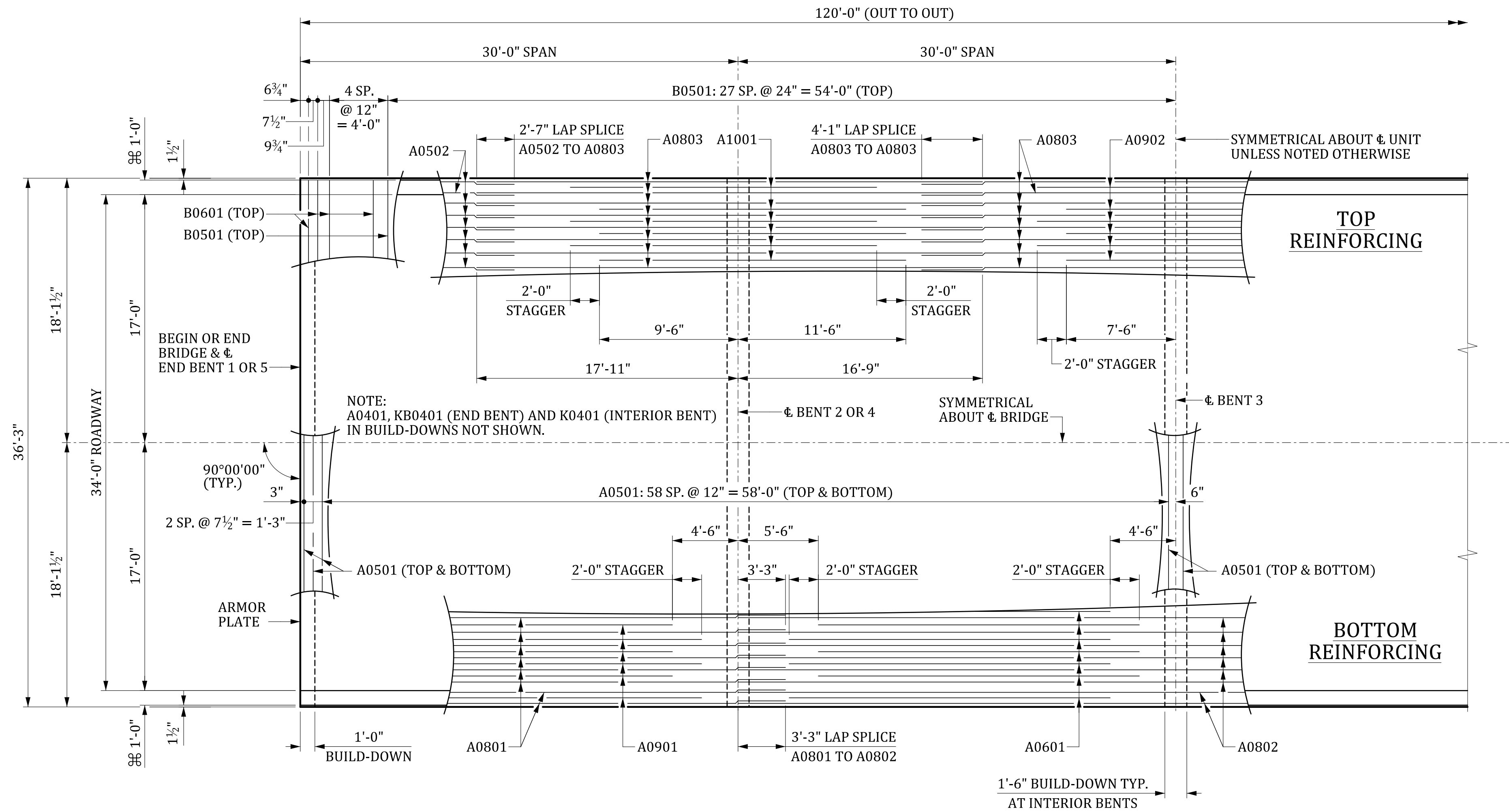
**120' FLAT SLAB UNIT (3 SPAN)
 SUPERSTRUCTURE
 (44'-0" ROADWAY)**

COUNTY: ##### ROUTE: #####

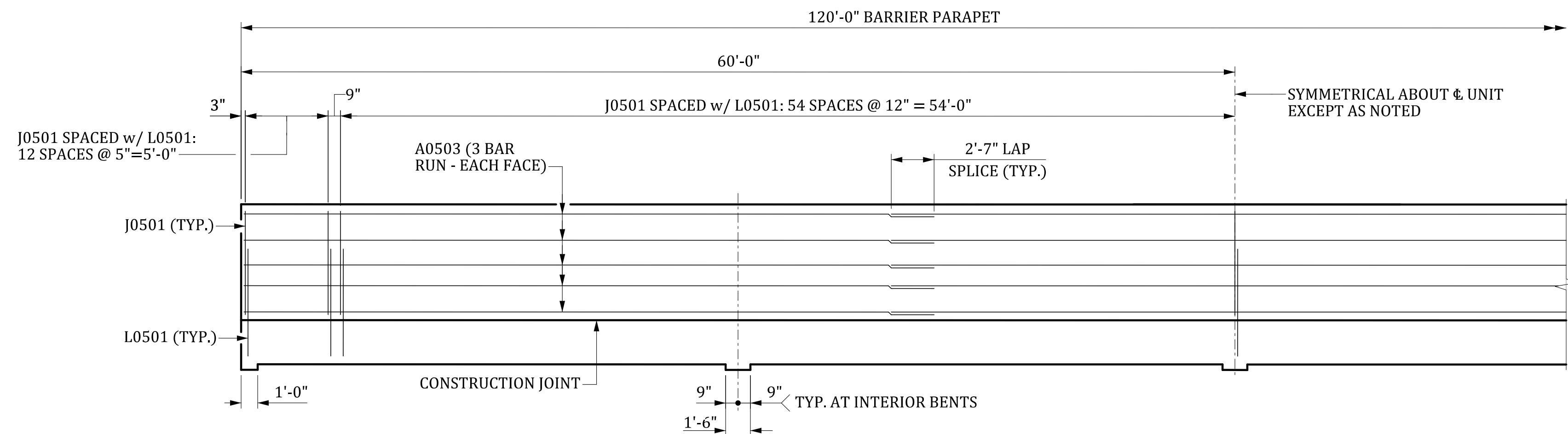
REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION

REVIEWED	CTM	GCM	11-23
QUAN.	CTM	MWB	11-23
DR.	SIA	DPP	10-23
DES.	BY	CHK.	DATE

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PARTIAL PLAN



PARTIAL ELEVATION

REINFORCING STEEL SCHEDULE							
MARK	NO. REQ'D	DIMENSION					LENGTH
		"a"	"b"	"c"	"d"	"e"	
A0401	10	19'-2"					19'-2"
A0501	244	35'-11"					35'-11"
A0502	78	14'-6"					14'-6"
A0503	60	41'-8"					41'-8"
A0601	72	22'-0"					22'-0"
A0801	78	33'-1"					33'-1"
A0802	39	60'-0"					60'-0"
A0803	117	34'-8"					34'-8"
A0901	72	25'-4"					25'-4"
A0902	36	17'-0"					17'-0"
A1001	72	21'-0"					21'-0"
B0501	110	5'-0"	7"				5'-7"
B0601	24	7'-0"	8"				7'-8"
J0501	270	5 1/2"	3'-3"				7'-0"
K0401	60	4 1/2"	1'-2"	1'-2"			4'-3"
KB0401	40	4 1/2"	1'-2"	8"			3'-9"
L0501	270	10"	3'-3"	6 1/4"	3'-3"		7'-11"
SB		2" HT.				AS NECESSARY	
CHCU		8" HT.				AS NECESSARY	
ESTIMATED QUANTITIES							
ITEM	UNIT	120' SPAN					
2.0" SCH. 80 PVC CONDUIT	LF	480.0					
CONCRETE, CLASS 4000	CY	217.5					
REINFORCING STEEL	LB	59,579					
MASH CONCRETE BARRIER PARAPET	LF	240.0					
ELASTOMERIC BEARING ASSEMBLY (FLAT SLAB)	EA	5					

NOTES:

CAST THE 120' CONTINUOUS UNIT IN ONE POUR FROM ONE END OF SPAN TO THE OTHER USING A SUITABLE SCREED. USE AN APPROVED RETARDING AGENT AND ESTABLISH AND MAINTAIN A MINIMUM POURING RATE OF 45 CY PER HOUR UNLESS APPROVED OTHERWISE BY THE RCE.

DO NOT REMOVE SLAB FALSEWORK UNTIL THE ENTIRE 120' SPAN UNIT HAS BEEN POURED AND CURED ACCORDING TO SECTION 702 OF THE STANDARD SPECIFICATIONS.

CAST BARRIER PARAPET CONCRETE AFTER SLAB FALSEWORK HAS BEEN STRUCK.

FOR ARMOR PLATE DETAIL, SEE "GENERAL DETAILS" SHEET.

FOR TYPICAL SECTION, SECTION THRU BARRIER PARAPET, SLAB BUILD-DOWN DETAILS AND DRAIN DETAILS, SEE "120' FLAT SLAB UNIT (4 SPAN) SUPERSTRUCTURE DETAILS" SHEET.

FOR SLAB DRAIN LOCATIONS, SEE "BRIDGE PLAN AND PROFILE" SHEET.

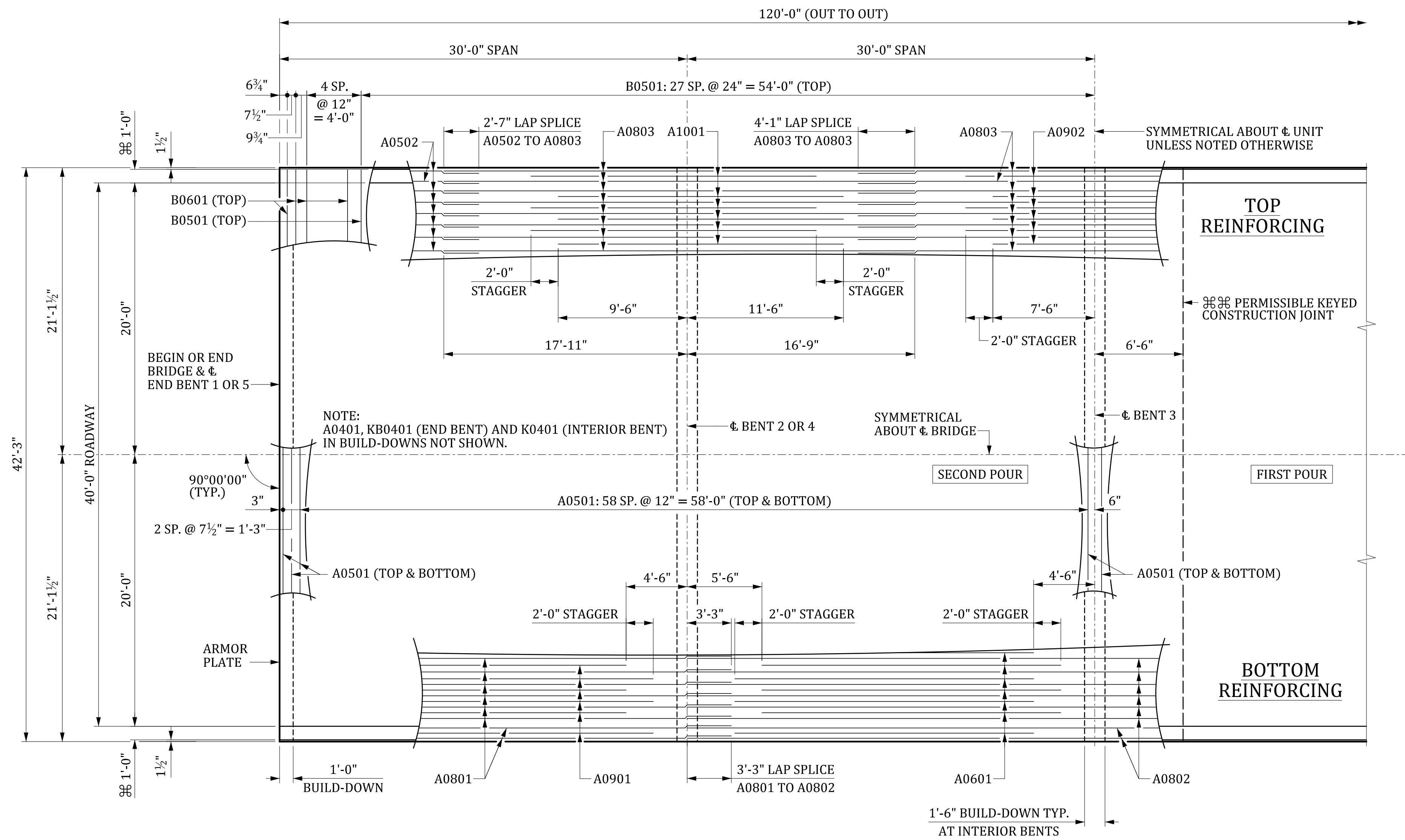
FOR REINFORCING BENDING DETAILS, SEE "REINFORCING BENDING DETAILS" SHEETS.

⊗ MASH BARRIER PARAPET

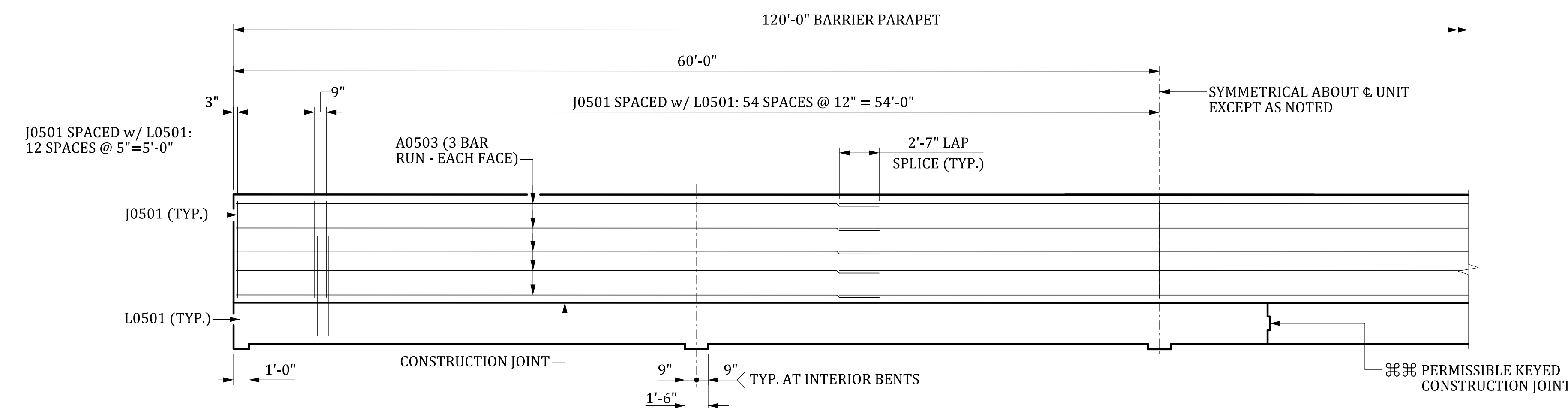
CONSULTANT NAME/LOGO
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION
120' FLAT SLAB UNIT (4 SPAN) SUPERSTRUCTURE (34'-0" ROADWAY)
COUNTY: #### ROUTE: ####

THIS DRAWING IS FURNISHED FOR INFORMATION ONLY. ALL DIMENSIONS SHOWN ARE SHEET SPECIFIC. ANY USE OF THIS DESIGN AND DRAWING, INCLUDING DIMENSIONS, MUST BE CHECKED BY THE USER'S ENGINEER TO ENSURE DESIGN IS ADEQUATE FOR THE INTENDED USE. ALL DRAWINGS MUST BE SIGNED AND SEALED BY A SOUTH CAROLINA REGISTERED PROFESSIONAL ENGINEER WHEN USED.

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REVIEWED	CTM	GCM	11-23
QUAN.	CTM	MWB	11-23
DR.	SJA	DPP	10-23
DES.	BY	CHK.	DATE



PARTIAL PLAN



PARTIAL ELEVATION

REINFORCING STEEL SCHEDULE							
MARK	NO. REQ'D	DIMENSION					LENGTH
		"a"	"b"	"c"	"d"	"e"	
A0401	10	25'-2"					25'-2"
A0501	244	41'-11"					41'-11"
A0502	90	14'-6"					14'-6"
A0503	60	41'-8"					41'-8"
A0601	84	22'-0"					22'-0"
A0801	90	33'-1"					33'-1"
A0802	45	60'-0"					60'-0"
A0803	135	34'-8"					34'-8"
A0901	84	25'-4"					25'-4"
A0902	42	17'-0"					17'-0"
A1001	84	21'-0"					21'-0"
B0501	110	5'-0"	7"				5'-7"
B0601	24	7'-0"	8"				7'-8"
J0501	270	5 1/2"	3'-3"				7'-0"
K0401	78	4 1/2"	1'-2"	1'-2"			4'-3"
KB0401	52	4 1/2"	1'-2"	8"			3'-9"
L0501	270	10"	3'-3"	6 1/4"	3'-3"		7'-11"
SB		2" HT.					AS NECESSARY
CHCU		8" HT.					AS NECESSARY
ESTIMATED QUANTITIES							
ITEM	UNIT	120' SPAN					
2.0" SCH. 80 PVC CONDUIT	LF	480.0					
CONCRETE, CLASS 4000	CY	253.8					
REINFORCING STEEL	LB	67,955					
MASH CONCRETE BARRIER PARAPET	LF	240.0					
ELASTOMERIC BEARING ASSEMBLY (FLAT SLAB)	EA	5					

NOTES:

CAST THE 120' CONTINUOUS UNIT IN ONE POUR FROM ONE END OF SPAN TO THE OTHER USING A SUITABLE SCREED. USE AN APPROVED RETARDING AGENT AND ESTABLISH AND MAINTAIN A MINIMUM POURING RATE OF 55 CY PER HOUR UNLESS APPROVED OTHERWISE BY THE RCE. IF A POURING RATE OF 55 CY PER HOUR IS NOT POSSIBLE, CAST THE 120' CONTINUOUS UNIT USING THE PERMISSIBLE KEYED CONSTRUCTION JOINT SHOWN.

DO NOT REMOVE SLAB FALSEWORK UNTIL THE ENTIRE 120' SPAN UNIT HAS BEEN POURED AND CURED ACCORDING TO SECTION 702 OF THE STANDARD SPECIFICATIONS.

CAST BARRIER PARAPET CONCRETE AFTER SLAB FALSEWORK HAS BEEN STRUCK.

FOR ARMOR PLATE DETAIL, SEE "GENERAL DETAILS" SHEET.

FOR TYPICAL SECTION, SECTION THRU BARRIER PARAPET, SLAB BUILD-DOWN DETAILS AND DRAIN DETAILS, SEE "120' FLAT SLAB UNIT (4 SPAN) SUPERSTRUCTURE DETAILS" SHEET.

FOR SLAB DRAIN LOCATIONS, SEE "BRIDGE PLAN AND PROFILE" SHEET.

FOR REINFORCING BENDING DETAILS, SEE "REINFORCING BENDING DETAILS" SHEETS.

⊗ MASH BARRIER PARAPET

⊗ ONLY ONE CONSTRUCTION JOINT IS ALLOWED IN THE 120' CONTINUOUS UNIT.

CONSULTANT NAME/LOGO

SOUTH CAROLINA
 DEPARTMENT OF TRANSPORTATION

**120' FLAT SLAB UNIT (4 SPAN)
 SUPERSTRUCTURE
 (40'-0" ROADWAY)**

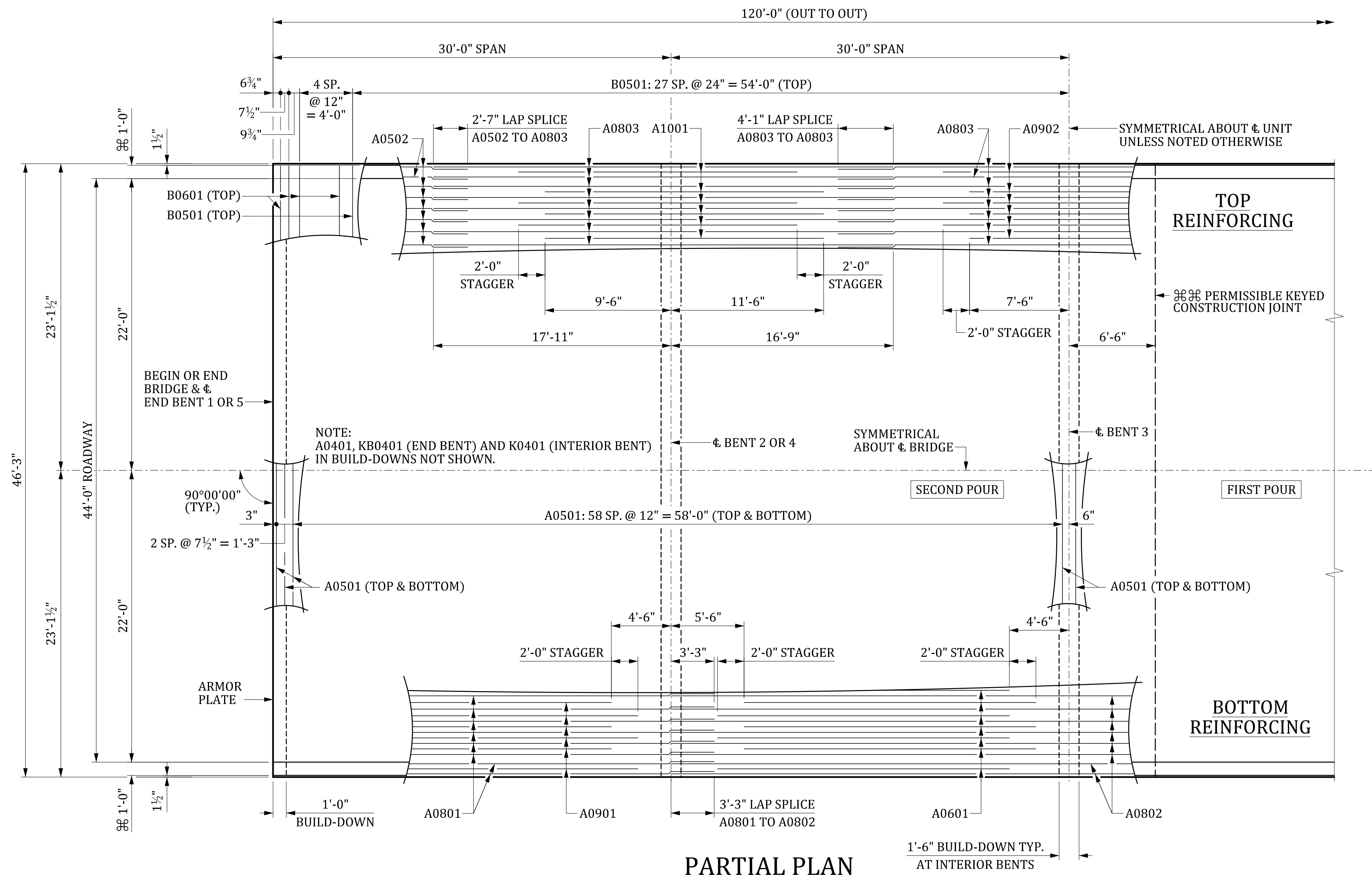
COUNTY: ##### ROUTE: #####

THIS DRAWING IS FURNISHED FOR INFORMATION ONLY. ALL DIMENSIONS SHOWN ARE SHEET SPECIFIC. ANY USE OF THIS DESIGN AND DRAWING, INCLUDING DIMENSIONS, MUST BE CHECKED BY THE USER'S ENGINEER TO ENSURE DESIGN IS ADEQUATE FOR THE INTENDED USE. ALL DRAWINGS MUST BE SIGNED AND SEALED BY A SOUTH CAROLINA REGISTERED PROFESSIONAL ENGINEER WHEN USED.

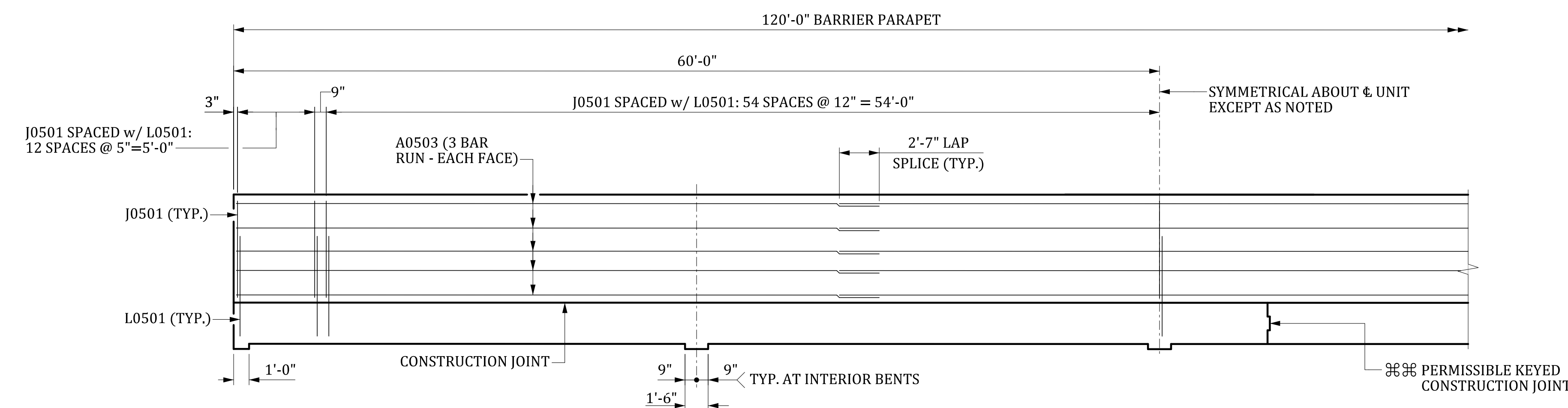
REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION

REVIEWED	CTM	GCM	11-23
QUAN.	CTM	MWB	11-23
DR.	SJA	DPP	10-23
DES.	BY	CHK.	DATE

MBreeland 6/26/2024 4:29:33 PM 702_FS_120.dgn



PARTIAL PLAN



PARTIAL ELEVATION

REINFORCING STEEL SCHEDULE							
MARK	NO. REQ'D	DIMENSION					LENGTH
		"a"	"b"	"c"	"d"	"e"	
A0401	10	29'-2"					29'-2"
A0501	244	45'-11"					45'-11"
A0502	98	14'-6"					14'-6"
A0503	60	41'-8"					41'-8"
A0601	92	22'-0"					22'-0"
A0801	98	33'-1"					33'-1"
A0802	49	60'-0"					60'-0"
A0803	147	34'-8"					34'-8"
A0901	92	25'-4"					25'-4"
A0902	46	17'-0"					17'-0"
A1001	92	21'-0"					21'-0"
B0501	110	5'-0"	7"				5'-7"
B0601	24	7'-0"	8"				7'-8"
J0501	270	5 1/2"	3'-3"				7'-0"
K0401	90	4 1/2"	1'-2"	1'-2"			4'-3"
KB0401	60	4 1/2"	1'-2"	8"			3'-9"
L0501	270	10"	3'-3"	6 1/4"	3'-3"		7'-11"
SB		2" HT.					AS NECESSARY
CHCU		8" HT.					AS NECESSARY

ESTIMATED QUANTITIES		
ITEM	UNIT	120' SPAN
2.0" SCH. 80 PVC CONDUIT	LF	480.0
CONCRETE, CLASS 4000	CY	278.0
REINFORCING STEEL	LB	73,541
MASH CONCRETE BARRIER PARAPET	LF	240.0
ELASTOMERIC BEARING ASSEMBLY (FLAT SLAB)	EA	5

NOTES:

CAST THE 120' CONTINUOUS UNIT IN ONE POUR FROM ONE END OF SPAN TO THE OTHER USING A SUITABLE SCREED. USE AN APPROVED RETARDING AGENT AND ESTABLISH AND MAINTAIN A MINIMUM POURING RATE OF 60 CY PER HOUR UNLESS APPROVED OTHERWISE BY THE RCE. IF A POURING RATE OF 60 CY PER HOUR IS NOT POSSIBLE, CAST THE 120' CONTINUOUS UNIT USING THE PERMISSIBLE KEYED CONSTRUCTION JOINT SHOWN.

DO NOT REMOVE SLAB FALSEWORK UNTIL THE ENTIRE 120' SPAN UNIT HAS BEEN POURED AND CURED ACCORDING TO SECTION 702 OF THE STANDARD SPECIFICATIONS.

CAST BARRIER PARAPET CONCRETE AFTER SLAB FALSEWORK HAS BEEN STRUCK.

FOR ARMOR PLATE DETAIL, SEE "GENERAL DETAILS" SHEET.

FOR TYPICAL SECTION, SECTION THRU BARRIER PARAPET, SLAB BUILD-DOWN DETAILS AND DRAIN DETAILS, SEE "120' FLAT SLAB UNIT (4 SPAN) SUPERSTRUCTURE DETAILS" SHEET.

FOR SLAB DRAIN LOCATIONS, SEE "BRIDGE PLAN AND PROFILE" SHEET.

FOR REINFORCING BENDING DETAILS, SEE "REINFORCING BENDING DETAILS" SHEETS.

⊗ MASH BARRIER PARAPET

⊗ ONLY ONE CONSTRUCTION JOINT IS ALLOWED IN THE 120' CONTINUOUS UNIT.

CONSULTANT NAME/LOGO

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

**120' FLAT SLAB UNIT (4 SPAN)
SUPERSTRUCTURE
(44'-0" ROADWAY)**

COUNTY: #### ROUTE: ####

THIS DRAWING IS FURNISHED FOR INFORMATION ONLY. ALL DIMENSIONS SHOWN ARE SHEET SPECIFIC. ANY USE OF THIS DESIGN AND DRAWING, INCLUDING DIMENSIONS, MUST BE CHECKED BY THE USER'S ENGINEER TO ENSURE DESIGN IS ADEQUATE FOR THE INTENDED USE. ALL DRAWINGS MUST BE SIGNED AND SEALED BY A SOUTH CAROLINA REGISTERED PROFESSIONAL ENGINEER WHEN USED.

REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION

REVIEWED	CTM	GCM	11-23
QUAN.	CTM	MWB	11-23
DR.	SJA	DPP	10-23
DES.	BY	CHK.	DATE

MBreeland 6/26/2024 4:29:34 PM 702_FS_120.dgn