

Submitted By: Anthony Lusk Date: ___/___/___ Recommended: Anthony Lusk Date: ___/___/___

Engineer of Record

To: Brian Klauk
Program / Project Manager

BASIS OF DESIGN EXCEPTION

- Request for Approval of Design Exceptions to AASHTO Guidelines
- Request for Approval of Design Exceptions from Standard SCDOT Procedures

PROJECT CHARACTERISTICS

County: LEXINGTON Rd./Route: I-20 Const. Pin: P043325

From: 61.5 To: 63.3

Length: 1.8 miles MPO / COG: COATS

Work Type: Widening of I-20

Functional Classification: Interstate

Group Designation: (1 / 2 / 3 / 4) (if applicable)

Type of Terrain: (Level / Rolling / Mountainous)

Design Speed: 60 (mph)

2019 ADT 84,000

2050 ADT 104,500

TRUCKS 5 %

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CRASH ANALYSIS

(Attach additional sheets with accident history data)

TOTAL PROJECT ESTIMATE (\$) 190,668,000.00

CHECK APPROPRIATE BOX(ES) FOR DESIGN EXCEPTION(S)

- | | | |
|---|--|--|
| <input type="checkbox"/> Design Speed | <input type="checkbox"/> Maximum Grade | <input type="checkbox"/> Travel Lane Width |
| <input type="checkbox"/> Horizontal Alignment | <input type="checkbox"/> Vertical Clearance | <input checked="" type="checkbox"/> Shoulder Width |
| <input type="checkbox"/> Minimum Radii | <input type="checkbox"/> Bridge Width | <input type="checkbox"/> Horizontal Clearance |
| <input type="checkbox"/> Vertical Alignment | <input type="checkbox"/> Structural Capacity | <input type="checkbox"/> Stopping Sight Distance |
| <input type="checkbox"/> Level SSD K-Values | <input type="checkbox"/> Superelevation Rate | |
| | <input type="checkbox"/> Cross Slope | |
| | <input type="checkbox"/> Travel Lanes | |
| | <input type="checkbox"/> Shoulders | |

DESCRIBE ELEMENT(S) FOR DESIGN EXCEPTION(S)

(Attach additional sheets as needed) Design Exception to allow narrow shoulders at limited locations to accommodate overhead sign supports. See Attachment A.

JUSTIFICATION FOR DESIGN EXCEPTION(S)

(Attach additional sheets as needed) See Attachment A.

DESCRIBE STEPS TO ELEMIMATE DESIGN EXCEPTION(S), INCLUDE COST

(Attach additional sheets as needed) See Attachment A.

HOW WILL FUTURE CONSTRUCTION IMPACT DESIGN EXCEPTION(S)?

(Attach additional sheets as needed) See Attachment A.

RECORD OF DECISION

For

Against

For

Against

Approved

Denied

_____/_____/_____
(Regional Design Manager/
Program Manager / DEA) Date

_____/_____/_____
(Regional Production Engineer) Date

_____/_____/_____
(Director of Preconstruction) Date

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Concur

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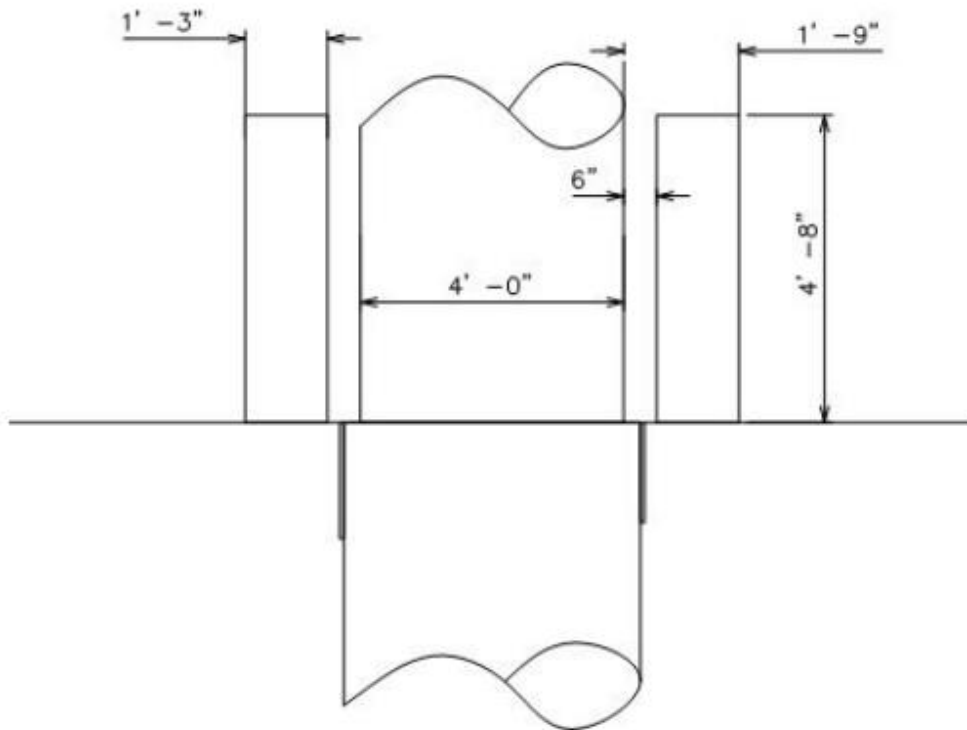
FHWA (NHS > \$50 million & All Interstate)

cc:
Director of Preconstruction
FHWA
Preconstruction Support Engineer
Regional Production Group Engineer
District Engineering Administrator
Director of Traffic Engineering

Attachment A – Design Exception

Describe Elements for Design Exception:

The Carolina Crossroads Phase 3C Project consists of all work necessary to complete the design and reconstruction of I-20 bridges over the Saluda River and CSX Railroad, along with associated I-20 widening between US-378 and Bush River Road (S-273) in Lexington County. Work will include design and construction of interstate widening, riverine bridges, railroad bridges, retaining walls, noise barrier wall, and related roadway appurtenances. This design exception would allow narrow shoulders at very limited locations (spot locations) to accommodate overhead sign supports that are constructed within the shoulder areas. Due to the location of the overhead structure support, the variance would be allowed in the inside shoulder. I-20 is a multilane interstate that uses concrete barrier to divide the directions of travel. The existing inside shoulder width along this route is approximately 4.75'. The proposed inside shoulder width is 12' along I-20. The minimum inside shoulder width for interstates with 3 or more lanes is 10'. Assuming an overhead sign structure foundation diameter of 4', the resulting inside shoulder width would be reduced to 9'-5". This design exception will allow shoulder width reductions at spot locations to be reduced to a minimum of 9'-5".



Justification for Design Exception

The design exception is requested to allow spot locations where the shoulder would be less than AASHTO requirements (approximately 9'-5" at a minimum) but greater than the existing 4.75'. Stopping sight distance would still be provided per the contract documents for the design-build project. A predictive crash analysis was performed by the Traffic Safety office in October 2019 as part of CCR Phase 2 and found no substantial increase in crash volumes between a 10' shoulder and 9' shoulder in either 5-lane or 4-lane sections.

Describe Steps to Eliminate Design Exception (include Costs):

To provide a 10' inside shoulder (AASHTO minimum), the crown point of I-20 in both directions would need to be shifted approximately 0.75' at spot locations. This would then result in deflections of the horizontal alignment and introduce curves or tapers into the mainline geometry which is undesirable. Other strategies such as reduction in lane width to provide shoulder width was ruled out due to the traffic volume and truck percentage. Providing 12' lanes on the interstates is more desirable than narrowing the lane widths. Costs associated with the elimination of the design exception are negligible, however, this design exception follows project precedence with Phase 1 and Phase 2 where inside shoulder width design exceptions were approved and provides consistent application of shoulder width treatments throughout the entire Carolina Crossroads program.

How will future Construction Impact Design Exception:

The proposed shoulder width design exception will maintain a consistent inside shoulder width for future widening and would not restrict future construction along the corridor.

October 14, 2019

TRAFFIC ENGINEERING SAFETY REVIEW

FROM: State Traffic Safety Engineer Smith

TO: Christopher Lacy

SUBJECT: Carolina Crossroads Project
Project ID P027662

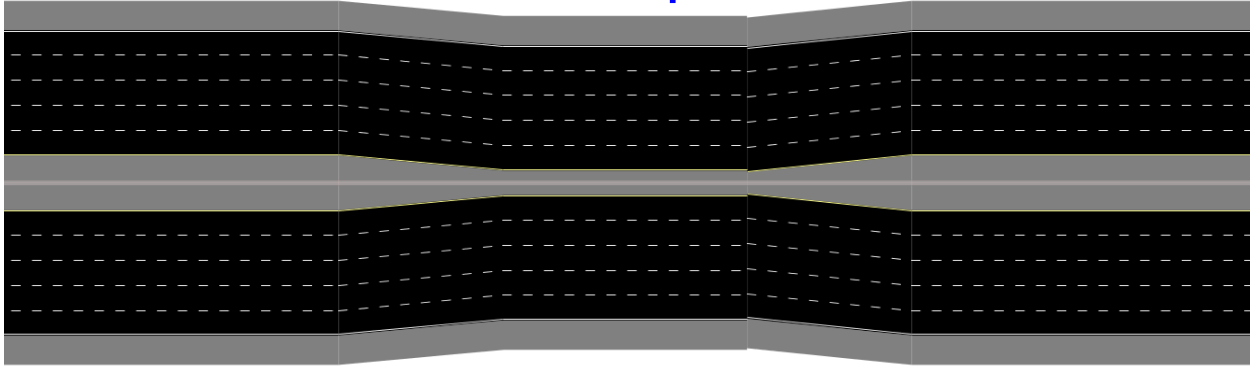
Safety Comments

The Traffic Safety Office has completed an analysis of predicted crashes in regards to reduced shoulder widths at point locations along the interstate corridors that comprise the Carolina Crossroads project. The analysis was conducted to provide data driven decisions regarding the safety impacts for various inside shoulder widths at interchanges and overpasses where guidance may be needed to save or replace existing structures.

As Carolina Crossroads is proceeding as a design build project, exact design elements and geometrics are not currently known, but a high level alternatives analysis using the IHSDM software could be conducted to determine impacts. A reduced inside shoulder width at a bridge was considered to be a 'point' along the corridor. It is understood that trying to determine the exact predicted crash rate at a single point along a corridor will not yield accurate results, and should only be used as a guide to review the trends as variables are changed. The results are not considered a full predicted analysis and lack the full input of data to complete a project specific HSM analysis. Additionally, results are for internal SCDOT use only in relation to this project and analysis only. These results should not be used to solely justify decisions on this project, or any project, without full review of all contextual elements involved within.

In order to complete the requested review, a sample interstate corridor similar to the proposed cross sections provided with the request was modeled. This sample model consisted of a 0.25 mile section of 4-lane interstate and 5-lane interstate. Projected AADT's were input as 90,000 and 100,000 VPD in the years 2020 and 2040 for the 4-lane section; along with 120,000 and 130,000 VPD for the 5-lane section. In order to get a baseline prediction of crashes for comparison, a maximum straight shoulder width of 12' was used to estimate a total number of crashes over a ten year period for the given conditions of each theoretical corridor. Subsequent crash totals were modeled by reducing the inside shoulder width for a section of the corridor to estimate the effects of a narrower inside shoulder at a point on the interstate due to interchange bridge piers and or barriers. The reduced shoulder widths were modeled for a length of approximately 225', of which 95' was the length of the fully reduced width, along with variable widths before and after this reduced width due to the leading and trailing tapers. These taper lengths started from a full 12' width shoulder, and tapered for a length of approximately 65'. The reduced shoulder widths that were sampled are as follows; 4.75', 6', 7', 8', 9', 9.75', 10', & 11' and are shown in the table below. Also shown is a sample plan view produced from the IHSDM software.

5-lane 12'-4.75' taper shoulders



Results from the Crash Prediction Module* of the IHSDM are as follows**:

5-Lane Interstate Corridor									
Reduced Shoulder Width at Bridge	4.75' (Existing)	6.0'	7.0'	8.0'	9.0'	9.75' (Proposed)	10.0' (Required)	11'	12' No reduction in shoulder width
Total Crashes (10 years)	70.9	70.7	70.6	70.5	70.4	70.3	70.2	70.1	69.9

4-Lane Interstate Corridor									
Reduced Shoulder Width at Bridge	4.75' (Existing)	6.0'	7.0'	8.0'	9.0'	9.75' (Proposed)	10.0' (Required)	11'	12' (No change)
Total Crashes (10 years)	55.8	55.6	55.5	55.4	55.3	55.2	55.2	55.1	55.0

*Complete Crash Prediction Module Reports showing further details and additional results are provided with this summary document for further review. **Based on SCDOT calibration and crash distributions

Summary

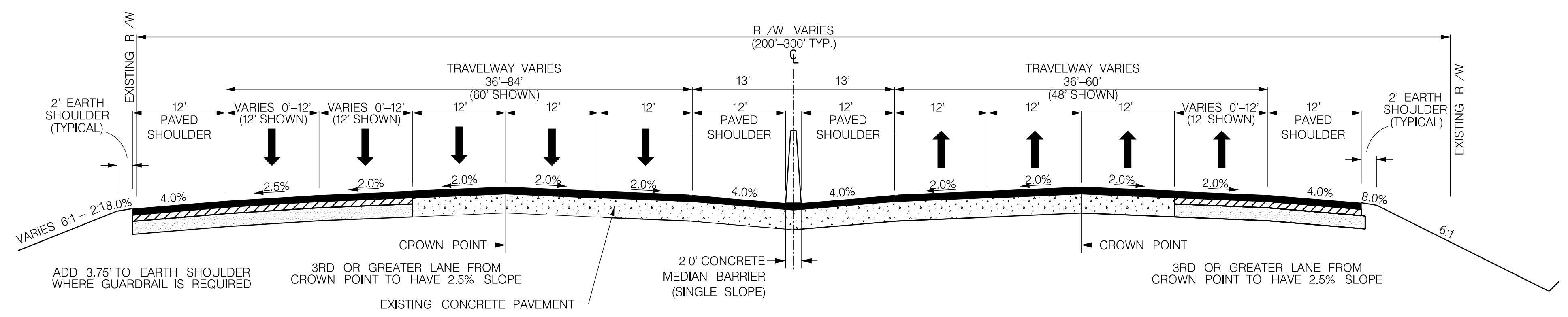
For the conditions outlined above and used in the IHSDM, results predict that there would be approximately 1 additional crash every 10 years in the worst case scenario of reducing the inside shoulder width from 12' to 4.75' for approximately 225'. There was practically no additional crash experience between the proposed 9.75' and minimum required width of 10.0'

Based upon these results with the provided input values, there does not appear to be any adverse safety effects at these point locations due to a reduction in shoulder widths.

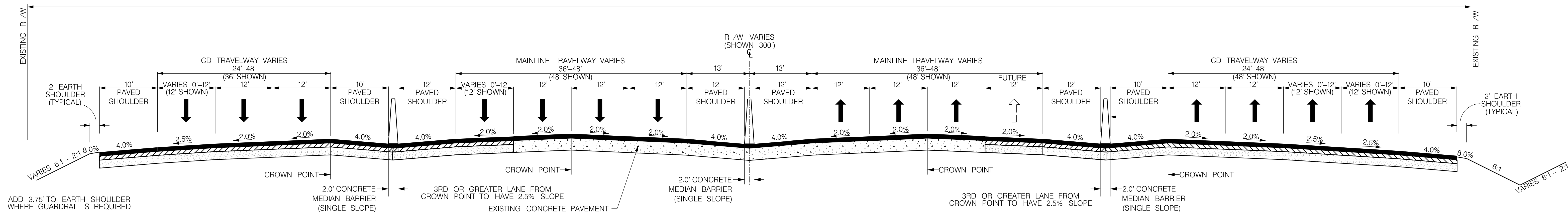
Duncan Smith

FOR INFORMATION ONLY

FED. ROAD DIST. NO.	STATE	COUNTY	PROJECT ID	RTE. NO.	SHEET NO.
3	S.C.	LEXINGTON RICHLAND	XX.XXXX		



INTERSTATE MAINLINE WITH MEDIAN BARRIER WALL
 INTERSTATE I-20 FC= URBAN INTERSTATE DS= 60 MPH
 STA. XXX + XX.XX - XXX + XX.XX



INTERSTATE MAINLINE WITH COLLECTOR-DISTRIBUTOR LANES
 INTERSTATE I-20 FC= URBAN INTERSTATE DS= 60 MPH
 STA. XXX + XX.XX - XXX + XX.XX

7/8/2020 HDR Engineering, Inc. of the Carolinas
 Carolina Crossroads_Typical Sections_PREFERRED_AL.T.dgn

RTE.	DESIGN SPEED	
	MPH	FROM STA. TO STA.

PAVEMENT DESIGN

APPROVED BY _____

DATE _____

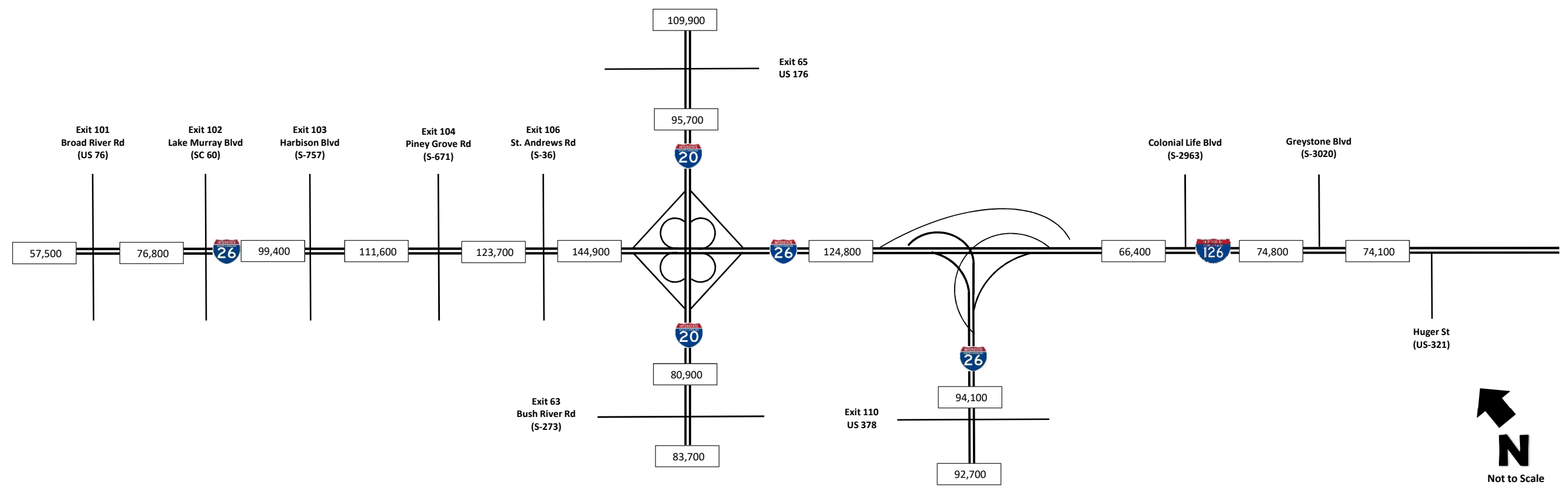
SOUTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 ROAD DESIGN COLUMBIA, S.C.

CAROLINA CROSSROADS
I-26/I-126/I-20
TYPICAL SECTIONS

SCALE: NTS RTE. DWG. NO.

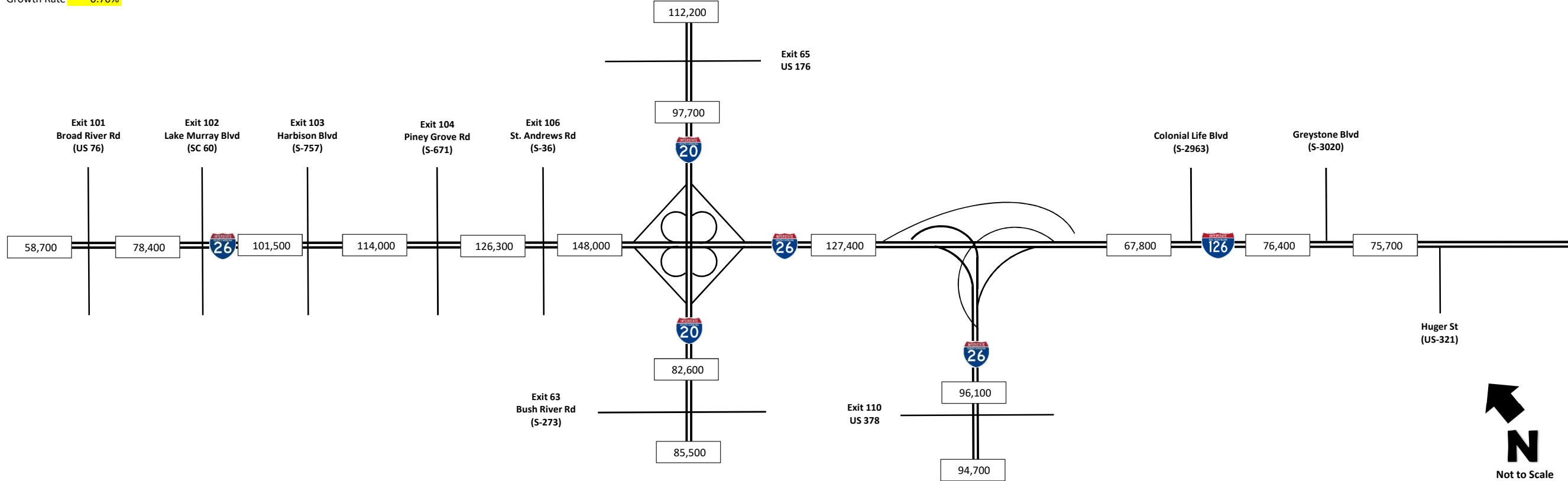
2017 FREEWAY SEGMENT AADT

Year **2017**



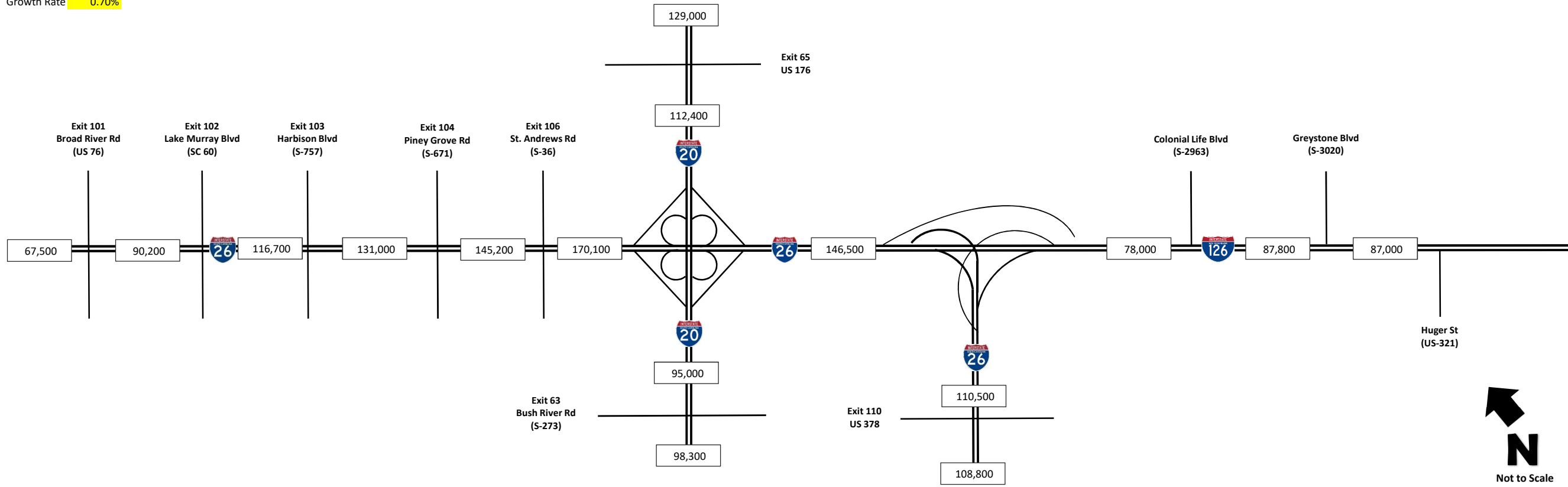
2020 ESTIMATED FREEWAY SEGMENT AADT

Design Year 2020
 Growth Rate 0.70%



2040 ESTIMATED FREEWAY SEGMENT AADT

Design Year 2040
Growth Rate 0.70%



Not to Scale

BUFILED	Cash Number	Cash Date	County	HDR_Zone	Route Type (Main)	Route Number (Main)	Route Aux (Main)	Route Name (Main)	Milepost	Route Type (Base)	Route Number (Base)	Route Aux (Base)	Route Name (Base)	Number Fatalities	Number Killed or Seriously Injured	Number Injured	Max Injury Code	Crash Harmful Event	Manner of Collision	Prme Contributing Factor	Crash Day of the Week	Rd Surface Conditions	Light Conditions	Speed Involved	OTFC Involved	Number of Units Involved	Unlabeled Total Count	Second Route Category Name	Sec:Second Route Auxiliary Name	Sec:3rd Name	DDO BA Latitude	Longitude	Main Route US	
3171	22277481	28-Aug-22	LEXINGTON	I-20	INTERSTATE	20		62.48 SECONDARY ROAD	273	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Animal (Deer Only)	Non Collision	Animal in Road	12:00	Thursday	Dry	Night	No	No	1	0	US ROUTE		34.02786	-81.13155	3201000200E	
2912	23055030	29-Dec-22	LEXINGTON	I-20	INTERSTATE	20		62.48 SECONDARY ROAD	273	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Animal (Deer Only)	Non Collision	Animal in Road	6:30	Thursday	Dry	Night	No	No	1	0	US ROUTE		34.02286	-81.13151	3201000200E	
796	2305850	30-Nov-23	LEXINGTON	I-20	INTERSTATE	20		62.549 SECONDARY ROAD	273	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Animal (Deer Only)	Non Collision	Animal in Road	13:00	Thursday	Dry	Daylight	No	No	1	0	US ROUTE		34.02299	-81.13081	3201000200E	
1009	23284601	6-Oct-23	LEXINGTON	I-20	INTERSTATE	20		62.677 SECONDARY ROAD	273	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Animal (Deer Only)	Non Collision	Animal in Road	22:05	Friday	Dry	Night	No	No	1	0	US ROUTE		34.02408	-81.12923	3201000200E	
23284602	23284602	6-Oct-23	LEXINGTON	I-20	INTERSTATE	20		62.738 SECONDARY ROAD	273	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Animal (Deer Only)	Non Collision	Animal in Road	22:05	Friday	Dry	Night	No	No	1	0	US ROUTE		34.02278	-81.12923	3201000200E	
6368	21240116	21-May-21	LEXINGTON	I-20	INTERSTATE	20		61.592 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Improper Lane use/Change	Non Collision	Improper Lane use/Change	11:30	Friday	Dry	Daylight	No	No	2	0	US ROUTE		34.01383	-81.14275	3201000200E	
3159	2291824	6-Nov-22	LEXINGTON	I-20	INTERSTATE	20		61.753 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Guardrail End	Non Collision	Driving too Fast for Conditions	15:35	Sunday	Wet	Daylight	No	No	1	0	SECONDARY ROAD		34.01256	-81.14054	3201000200E	
5208	21462087	24-Dec-21	LEXINGTON	I-20	INTERSTATE	20		62.113 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	3 Possible Injury	Guardrail End	Non Collision	Improper Lane use/Change	8:44	Friday	Dry	Daylight	No	No	1	0	SECONDARY ROAD		34.01903	-81.13605	3201000200E	
4340	22217910	3-Mar-22	LEXINGTON	I-20	INTERSTATE	20		62.264 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	1 Possible Injury	Guardrail End	Non Collision	Driving too Fast for Conditions	7:25	Thursday	Dry	Daylight	No	No	2	0	SECONDARY ROAD		34.02954	-81.13426	3201000200E	
0953	2221597	13-Feb-22	LEXINGTON	I-20	INTERSTATE	20		61.911 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	1 Possible Injury	Guardrail End	Non Collision	Driving too Fast for Conditions	11:30	Friday	Dry	Night	No	No	1	0	SECONDARY ROAD		34.01979	-81.13426	3201000200E	
1053	23281822	5-Oct-23	LEXINGTON	I-20	INTERSTATE	20		62.264 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Guardrail Face	Angle 3	Driving too Fast for Conditions	19:55	Thursday	Dry	Daylight	Yes	No	2	0	SECONDARY ROAD		34.02035	-81.13453	3201000200E	
1947	23227099	15-Jan-23	LEXINGTON	I-20	INTERSTATE	20		61.76 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	No Injury	Guardrail Face	Non Collision	Improper Lane use/Change	11:50	Wednesday	Dry	Night	No	No	2	0	SECONDARY ROAD		34.01533	-81.14042	3201000200E	
4479	22208590	22-Mar-22	LEXINGTON	I-20	INTERSTATE	20		62.74 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Guardrail Face	Non Collision	Driving too Fast for Conditions	7:11	Saturday	Wet	Night	No	No	1	0	US ROUTE		34.02549	-81.1284	3201000200E
4336	21242923	6-CONV-22	LEXINGTON	I-20	INTERSTATE	20		62.903 SECONDARY ROAD	273	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Guardrail Face	Non Collision	Driving too Fast for Conditions	11:20	Monday	Wet	Night	No	No	1	0	SECONDARY ROAD		34.02274	-81.13426	3201000200E
3744	22253838	3-Jul-22	LEXINGTON	I-20	INTERSTATE	20		62.983 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Guardrail Face	Non Collision	Driving too Fast for Conditions	18:35	Sunday	Wet	Daylight	No	No	1	0	US ROUTE		34.02796	-81.12552	3201000200E
4445	2221329	31-Jan-22	LEXINGTON	I-20	INTERSTATE	20		62.479 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Guardrail Face	Sideways, Same Direction	Improper Lane use/Change	10:32	Monday	Dry	Daylight	No	No	2	0	US ROUTE		34.02276	-81.13167	3201000200E
4644	21279849	4-Oct-21	LEXINGTON	I-20	INTERSTATE	20		62.538 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Guardrail Face	Sideways, Same Direction	Improper Lane use/Change	21:30	Monday	Dry	Night	No	No	2	0	US ROUTE		34.02238	-81.13094	3201000200E
5476	22208823	24-Jan-22	LEXINGTON	I-20	INTERSTATE	20		62.391 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Highway Traffic Sign Post	Non Collision	Driving too Fast for Conditions	20:20	Monday	Dry	Night	No	No	2	0	US ROUTE		34.01476	-81.13775	3201000200E
0566	23221546	31-Mar-22	LEXINGTON	I-20	INTERSTATE	20		62.411 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Angle 3	Non Collision	Driving too Fast for Conditions	21:40	Monday	Wet	Night	No	No	2	0	SECONDARY ROAD		34.02278	-81.13262	3201000200E
4219	2224726	29-Mar-22	LEXINGTON	I-20	INTERSTATE	20		62.752 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Angle 3	Improper Lane use/Change	13:40	Tuesday	Dry	Daylight	No	No	3	0	US ROUTE		34.02558	-81.12931	3201000200E
595	23314048	17-Dec-23	LEXINGTON	I-20	INTERSTATE	20		61.58 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Improper Lane use/Change	12:31	Sunday	Wet	Daylight	No	No	2	0	SECONDARY ROAD		34.01374	-81.14293	3201000200E
3387	2272961	25-Sep-22	LEXINGTON	I-20	INTERSTATE	20		61.645 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	3:35	Sunday	Dry	Night	No	No	1	0	SECONDARY ROAD		34.01428	-81.142	3201000200E
1863	2323283	22-Apr-23	LEXINGTON	I-20	INTERSTATE	20		61.661 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Improper Lane use/Change	Non Collision	Improper Lane use/Change	4:10	Wednesday	Dry	Night	No	No	2	0	SECONDARY ROAD		34.01842	-81.14296	3201000200E
3006	21259438	19-Aug-21	LEXINGTON	I-20	INTERSTATE	20		61.745 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	20:50	Thursday	Wet	Night	No	No	1	0	SECONDARY ROAD		34.01519	-81.14065	3201000200E
3152	2292249	27-Nov-22	LEXINGTON	I-20	INTERSTATE	20		61.758 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	4:40	Sunday	Wet	Night	Yes	No	1	0	SECONDARY ROAD		34.01435	-81.14047	3201000200E
5804	21269368	28-Aug-21	LEXINGTON	I-20	INTERSTATE	20		61.966 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	11:50	Sunday	Wet	Daylight	No	No	1	0	SECONDARY ROAD		34.01752	-81.13792	3201000200E
5632	21268253	17-Aug-21	LEXINGTON	I-20	INTERSTATE	20		61.98 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	5:30	Thursday	Dry	Daylight	No	No	1	0	SECONDARY ROAD		34.01779	-81.13775	3201000200E
6161	2125927	26-Jul-21	LEXINGTON	I-20	INTERSTATE	20		61.989 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	19:20	Monday	Wet	Daylight	No	No	1	0	SECONDARY ROAD		34.01779	-81.13762	3201000200E
2940	22207332	15-Dec-22	LEXINGTON	I-20	INTERSTATE	20		62.055 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	23:00	Thursday	Dry	Daylight	No	No	2	0	SECONDARY ROAD		34.01835	-81.13685	3201000200E
1495	2325367	26-Jun-23	LEXINGTON	I-20	INTERSTATE	20		62.135 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	7:00	Monday	Wet	Night	No	No	1	0	SECONDARY ROAD		34.01912	-81.13585	3201000200E
2383	2281525	25-Oct-22	LEXINGTON	I-20	INTERSTATE	20		62.157 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	7:45	Tuesday	Dry	Night	Yes	No	1	0	SECONDARY ROAD		34.01943	-81.13556	3201000200E
4524	22205205	19-Nov-22	LEXINGTON	I-20	INTERSTATE	20		62.201 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	7:40	Monday	Wet	Daylight	No	No	1	0	SECONDARY ROAD		34.01989	-81.13419	3201000200E
4324	22218852	12-Mar-22	LEXINGTON	I-20	INTERSTATE	20		62.329 US ROUTE	378	MAINLINE	SUNSET BLVD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	7:50	Monday	Wet	Night	No	No	1	0	SECONDARY ROAD		34.02121	-81.13349	3201000200E
2942	22306831	17-Dec-22	LEXINGTON	I-20	INTERSTATE	20		62.506 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Tires/Wheel	13:35	Saturday	Dry	Daylight	No	No	1	0	US ROUTE		34.02305	-81.13133	3201000200E
4482	22208467	29-Jan-22	LEXINGTON	I-20	INTERSTATE	20		62.636 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	13:20	Saturday	Dry	Night	No	No	1	0	US ROUTE		34.02442	-81.12927	3201000200E
4510	22206908	22-Jan-22	LEXINGTON	I-20	INTERSTATE	20		62.776 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	6:30	Saturday	Wet	Night	No	No	1	0	US ROUTE		34.02584	-81.12796	3201000200E
4465	22209195	22-Jan-22	LEXINGTON	I-20	INTERSTATE	20		62.787 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	12:00	Monday	Wet	Night	No	No	1	0	SECONDARY ROAD		34.02004	-81.13262	3201000200E
1943	23227549	10-Mar-23	LEXINGTON	I-20	INTERSTATE	20		62.901 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD		BUSH RIVER RD	0	0	0	0	No Injury	Median Barrier	Non Collision	Driving too Fast for Conditions	11:30	Friday	Wet	Daylight	No	No	1	0	US ROUTE		34.02712	-81.12649	3201000200E

5317	21455608	29-Nov-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.525 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	7:15	Monday	Dry	Daylight	No	No	3	0	US ROUTE	378	MAINLINE	SUNSET BLVD	100	W	34.02524	-81.1311	32010002000E
1107	23272399	12-Sep-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.965 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	8:15	Tuesday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	80	W	34.02468	-81.12638	32010002000E
4944	21532348	16-Apr-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.796 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	22:50	Friday	Dry	Night	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	100	E	34.02603	-81.12777	32010002000E
5841	21288911	26-Oct-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.829 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	17:10	Tuesday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	59	W	34.02637	-81.12736	32010002000E
1562	23249658	12-Jun-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.875 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	17:30	Monday	Dry	Daylight	No	No	3	0	US ROUTE	378	MAINLINE	SUNSET BLVD	50	W	34.02685	-81.1268	32010002000E
1779	23235799	4-May-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.861 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	7:40	Thursday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	87	E	34.02691	-81.12673	32010002000E
1761	23232797	3-May-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.884 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	7:55	Wednesday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	54	E	34.02694	-81.12685	32010002000E
1961	23226583	24-Mar-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.896 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	17:05	Friday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	100	E	34.02707	-81.12655	32010002000E
758	23300311	8-Nov-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.927 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	1	Possible Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	7:20	Wednesday	Dry	Daylight	No	No	3	0	US ROUTE	378	MAINLINE	SUNSET BLVD	48	W	34.02739	-81.12617	32010002000E
3110	22294698	19-Nov-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.98 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	12:45	Saturday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	40	E	34.02795	-81.12554	32010002000E
3710	22256144	6-Nov-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.982 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	2	Minor Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	15:45	Sunday	Wet	Daylight	No	No	3	0	US ROUTE	378	MAINLINE	SUNSET BLVD	18	E	34.02798	-81.12552	32010002000E
3106	22295267	21-Nov-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.993 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	2	Possible Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	17:09	Monday	Dry	Night	No	No	3	0	US ROUTE	378	MAINLINE	SUNSET BLVD	40	W	34.02809	-81.12539	32010002000E
4209	22225439	30-Mar-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	63.058 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	17:00	Wednesday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	20	W	34.02877	-81.1246	32010002000E
3339	22280787	18-Oct-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	63.086 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	7:50	Tuesday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	25	W	34.02906	-81.12426	32010002000E
1969	23225947	26-Mar-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	63.116 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Rear End	Driving too Fast for Conditions	7:55	Tuesday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	25	W	34.02939	-81.12367	32010002000E
4466	22209162	26-Jan-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.467 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	1	Possible Injury	Motor Unit (Stopped)	Sideswipe, Same Direction	Driving too Fast for Conditions	17:25	Wednesday	Dry	Daylight	No	No	3	0	US ROUTE	378	MAINLINE	SUNSET BLVD	102	W	34.02285	-81.13156	32010002000E
1806	23234708	29-Mar-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.958 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Motor Unit (Stopped)	Sideswipe, Same Direction	Driving too Fast for Conditions	8:10	Wednesday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	50	W	34.02772	-81.1258	32010002000E
5435	21294552	11-Nov-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	61.66 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	17:20	Thursday	Wet	Night	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	52	E	34.01441	-81.14179	32010002000E
5663	21276881	27-Sep-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	61.901 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	6:19	Monday	Dry	Night	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	83	E	34.01678	-81.13871	32010002000E
3903	22344007	26-Jun-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	61.98 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	11:30	Saturday	Dry	Daylight	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	100	E	34.0176	-81.13775	32010002000E
4330	22216499	7-Mar-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.003 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	15:45	Monday	Dry	Daylight	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	94	E	34.01784	-81.13747	32010002000E
1066	23280271	31-Aug-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.036 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Other Movable Object	Non Collision	Cargo	18:50	Thursday	Dry	Daylight	No	No	2	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	110	E	34.01818	-81.13706	32010002000E
5846	21267200	8-Oct-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.152 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	6:30	Friday	Dry	Night	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	30	W	34.01937	-81.13365	32010002000E
6456	21236143	29-Apr-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.591 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	16:20	Thursday	Dry	Daylight	No	No	4	0	US ROUTE	378	MAINLINE	SUNSET BLVD	15	W	34.02393	-81.1303	32010002000E
3756	22253349	8-Jul-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.755 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	6:00	Friday	Dry	Daylight	No	No	1	0	US ROUTE	378	MAINLINE	SUNSET BLVD	69	W	34.02561	-81.12827	32010002000E
6283	21244084	11-Jun-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.809 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	3:17	Friday	Wet	Night	No	No	1	0	US ROUTE	378	MAINLINE	SUNSET BLVD	50	W	34.02616	-81.12761	32010002000E
2102	23218799	15-Feb-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.888 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Other Movable Object	Non Collision	Debris	22:30	Wednesday	Dry	Night	No	No	1	0	US ROUTE	378	MAINLINE	SUNSET BLVD	20	W	34.02699	-81.12664	32010002000E
3531	22266545	27-Aug-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	63.009 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Other Movable Object	Non Collision	Other Improper Action	17:50	Saturday	Dry	Daylight	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	9	W	34.02627	-81.1252	32010002000E
1711	23340589	9-May-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	63.021 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	1	Minor Injury	Other Movable Object	Non Collision	Debris	14:00	Tuesday	Dry	Daylight	No	No	1	0	US ROUTE	378	MAINLINE	SUNSET BLVD	37	W	34.02539	-81.12659	32010002000E
5392	21452113	8-Dec-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.982 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	2	Possible Injury	Overturn/Rollover	Angle 3	Driving too Fast for Conditions	6:35	Wednesday	Wet	Night	No	No	2	0	US ROUTE	378	MAINLINE	SUNSET BLVD	20	W	34.02798	-81.12552	32010002000E
4197	22225840	12-Apr-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.267 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Ran off Road Right	Non Collision	Cargo	12:00	Tuesday	Dry	Daylight	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	128	E	34.02657	-81.13424	32010002000E
532	23314690	26-Dec-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	61.816 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Tree	Non Collision	Driving too Fast for Conditions	20:03	Tuesday	Wet	Night	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	83	W	34.0159	-81.13875	32010002000E
4256	22222237	13-Mar-22	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	61.825 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Tree	Non Collision	Steering	16:25	Sunday	Dry	Daylight	Yes	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	100	W	34.01599	-81.13964	32010002000E
1746	23237291	18-Apr-23	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	61.925 US ROUTE	378	MAINLINE	SUNSET BLVD	0	1	1	Serious Injury	Tree	Non Collision	Medical Related	17:01	Tuesday	Dry	Daylight	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	11	E	34.01703	-81.13842	32010002000E
5194	21463033	19-Dec-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	61.98 US ROUTE	378	MAINLINE	SUNSET BLVD	0	0	0	No Injury	Tree	Non Collision	Driving too Fast for Conditions	5:01	Sunday	Wet	Daylight	No	No	1	0	SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	100	E	34.0176	-81.13775	32010002000E
6231	21247640	11-Jun-21	LEXINGTON	I-20	INTERSTATE	20	MAINLINE	INTERSTATE 20	62.404 SECONDARY ROAD	273	MAINLINE	BUSH RIVER RD	0	0	0	No Injury	Tree	Non Collision	Obstruction																	